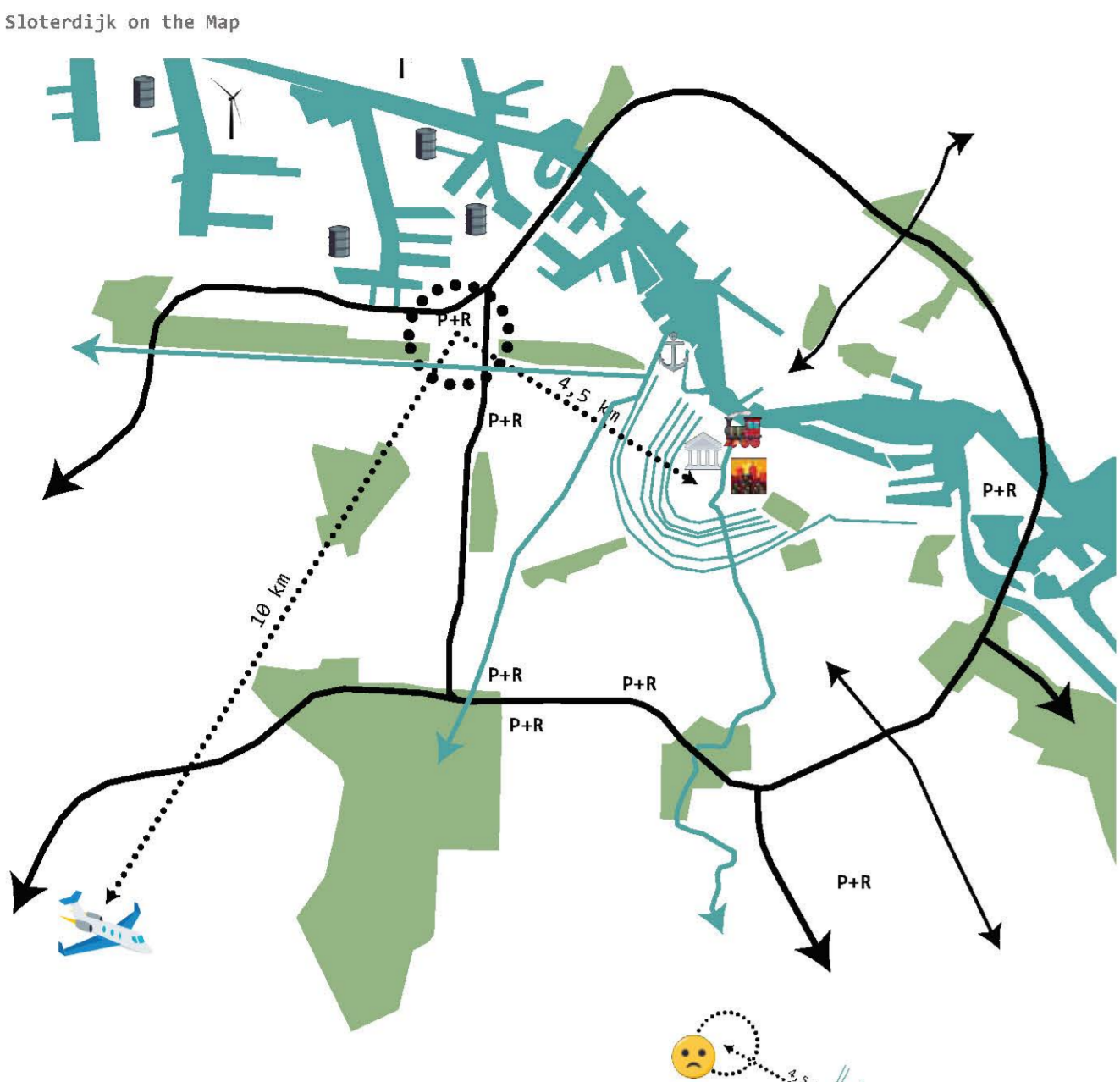
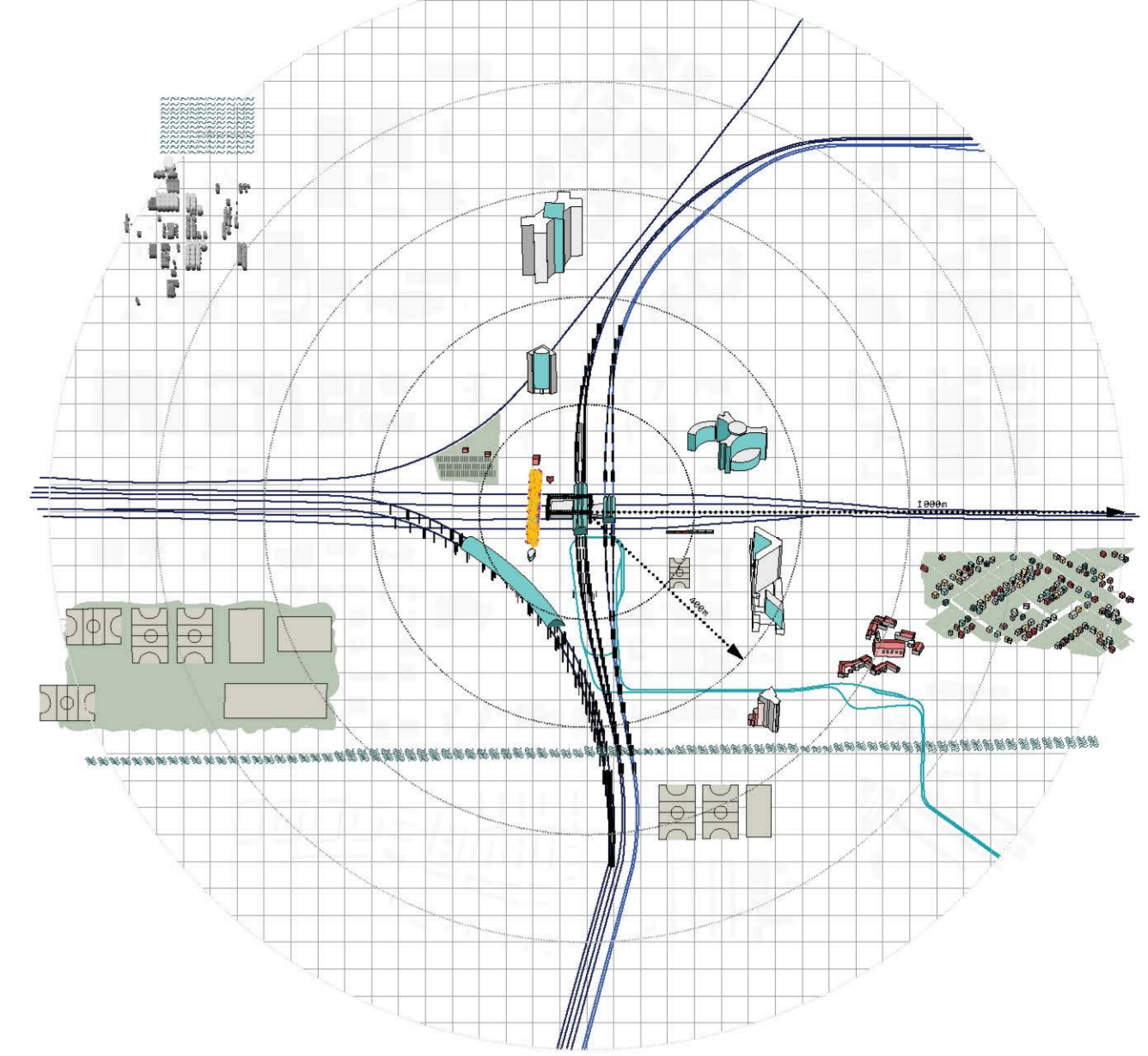


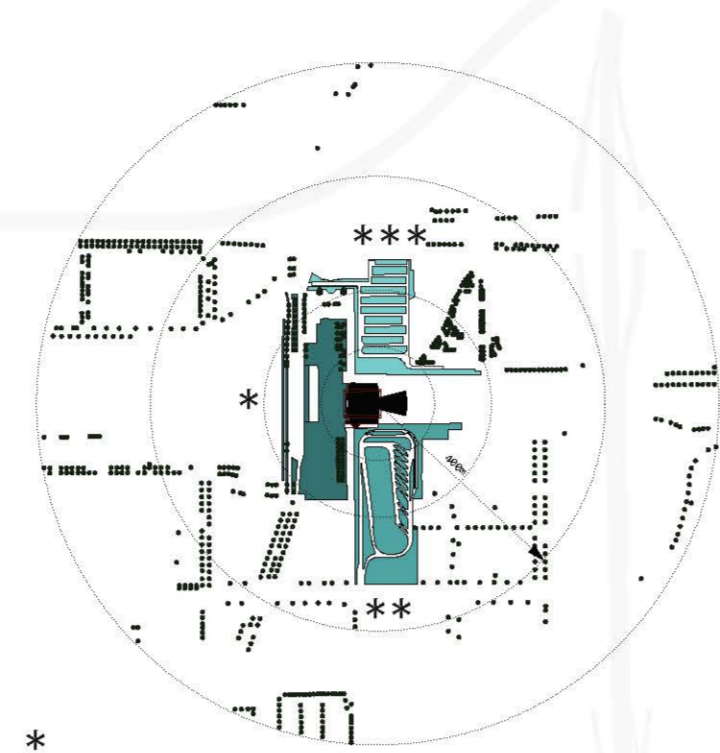
Or is it: Happy People - Productive People?

All professions that are related to shaping our cities, starting from the architect to the sociologist, from urbanist to politicians or from university to developer etc. tend to think and design based on numbers, statistics, rates, places, actions, needs and problems.

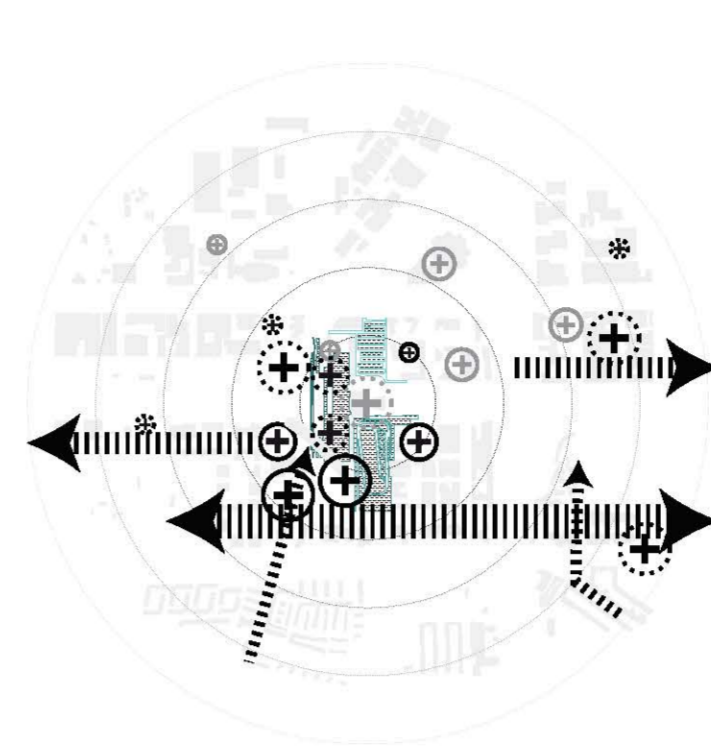
Urban Topography



Existing Character of the 3 Squares



Updating Sloterdijk



- \* Oriplein - Regional Character
- Related to Randstad
- Openness in the Landscape
- Main entrance for the train station
- Leisure/ Recreation
\*\* Carrascoplein - Urban Character
- Related to the city
- Concentration in the Landscape
- Secondary entrances for the train station
- Infrastructure
\*\*\* Piarcopein - Local Character
- Disconnected from the urban fabric
- Almost no landscape elements
- Tertiary entrances for the train station
- Parking
\*\*\*\* Piarcopein - Local/ Urban/ Regional Character
- Related to the surrounding buildings & program
- To the city, to the new developments, to the station, to the port, to old Sloterdijk, to the green zones, to HAPPINESS.
- Openness AND concentration in the Landscape
- Urban forest & green recreational spaces.
- Secondary entrances for the train station related to the new developments regarding Moona-genda & Koers 2025
- Innovation/ PRODUCTION

What about Sloterdijk?

Before focusing on Sloterdijk itself, we find reasonable to mention some facts and rankings about the city of Amsterdam in general and the Netherlands as well.

Amsterdam ranks 1st in the world as the best city for millennials. Amsterdam ranks 4th of Europe's most dynamic cities. Amsterdam ranks 1st as city with most cultural attractions per capita in the world.

The Netherlands ranks 8th globally with the best quality of life and 9th at the best countries in the world for green living. The Netherlands ranks 6th in the world Happiness Report.

Where does Sloterdijk stand in these rankings?

Productive Sloterdijk

Sloterdijk is productive and always has been! Productivity exists in manufacturing, industry, office, port, education. The problem is though that they are isolated from each other. The spatial relations are based on "separation and unlimited resources".

Happy Sloterdijk

Sloterdijk is not happy! You can feel it even when you travel by train and you are above the viaducts that pass over Piarcopein. The public space is a lot but unorganized and unfriendly, cars and infrastructures dominate the area.

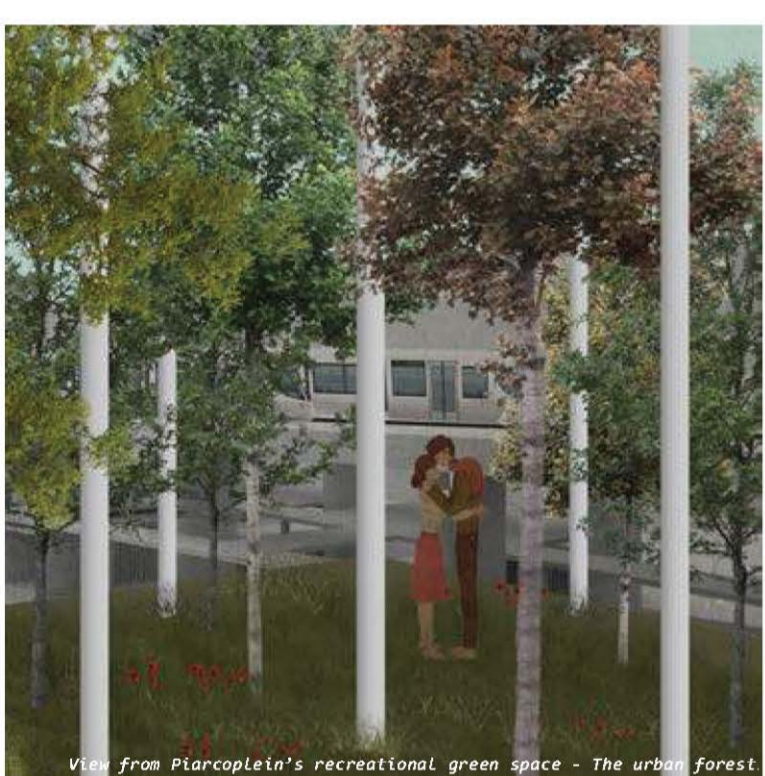
4 Design Principles

Mobility



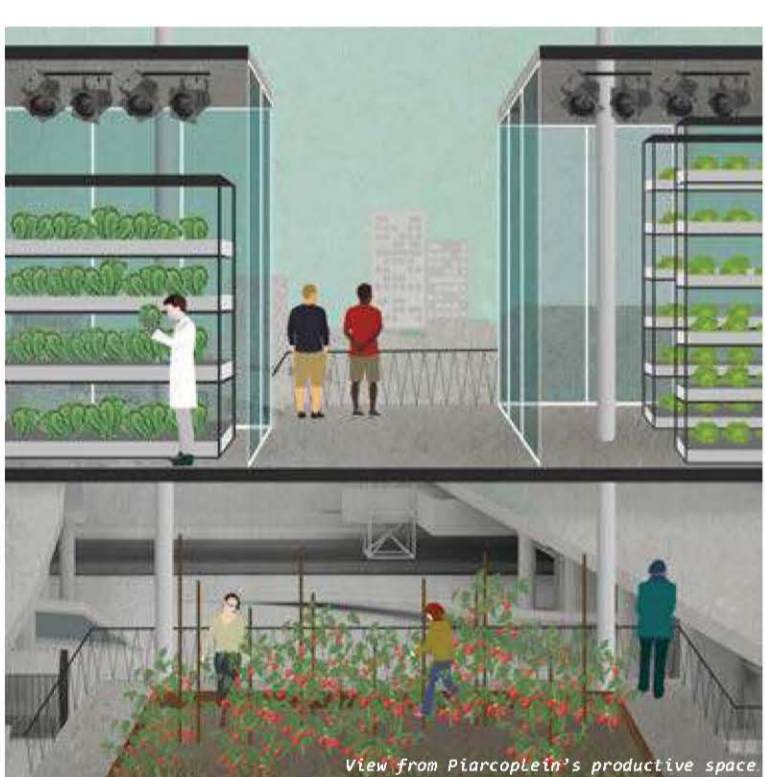
Networks that allow different types of mobility and infrastructure to co-exist flexibly within the city are the drivers of city life and introduce a "dynamic economy" in the city.

Nature



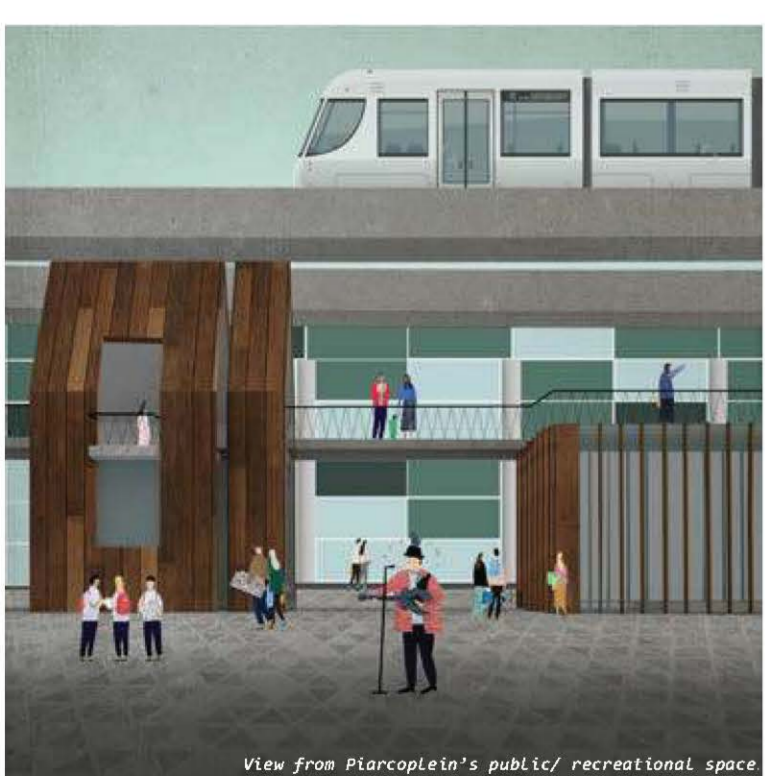
It is widely proved that green urban space and connection with nature are linked to the factor of happiness, ecological sustainability and social well-being.

Inclusiveness



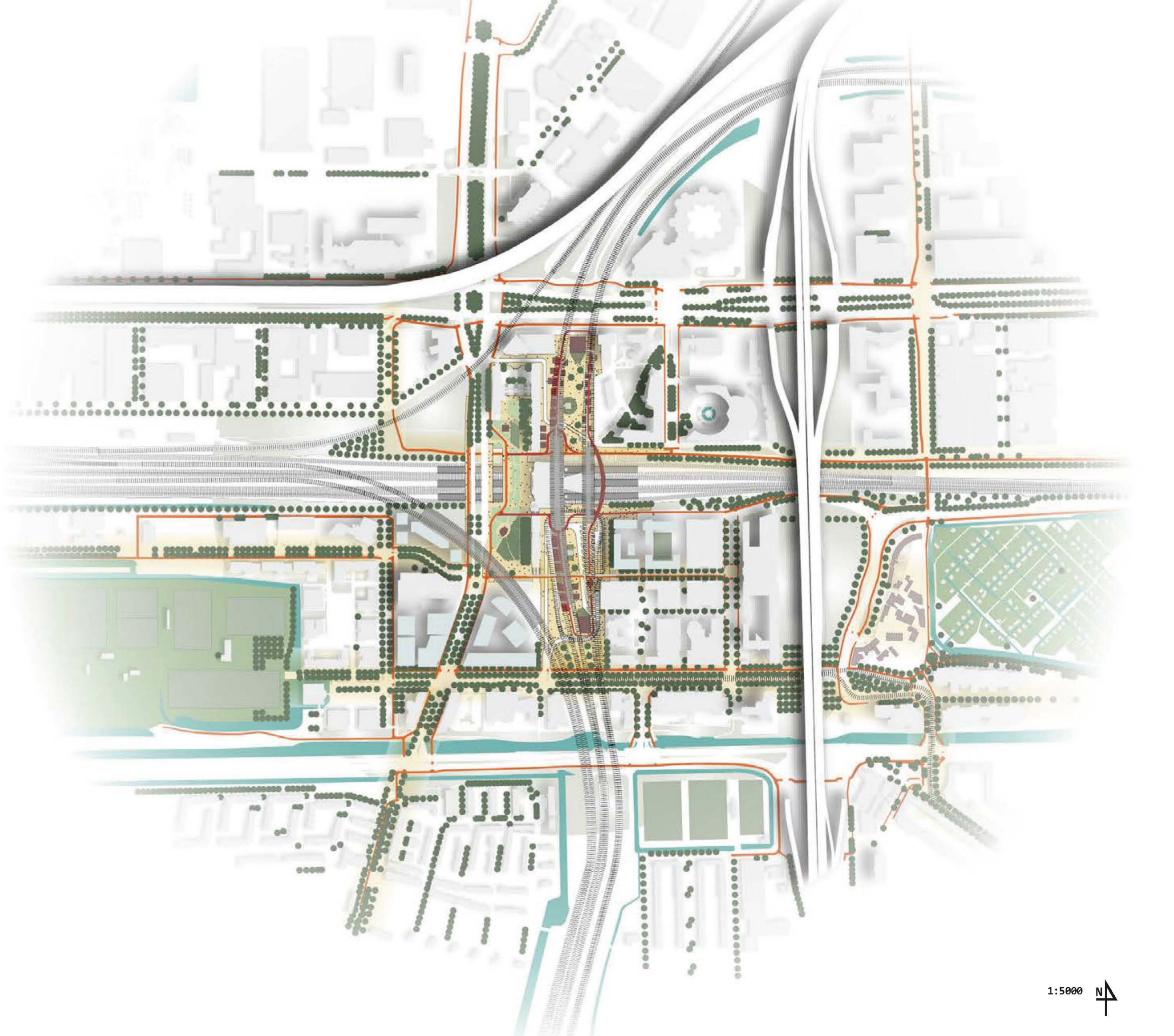
To connect the city is to generate culture, to allow interactivity, to provide safety, to raise the social value of the street and finally to accommodate all kind of users.

Urban Image



A successful city is also a well-connected city. Connection is related to the systems that are applied in designing and building the cities of tomorrow.

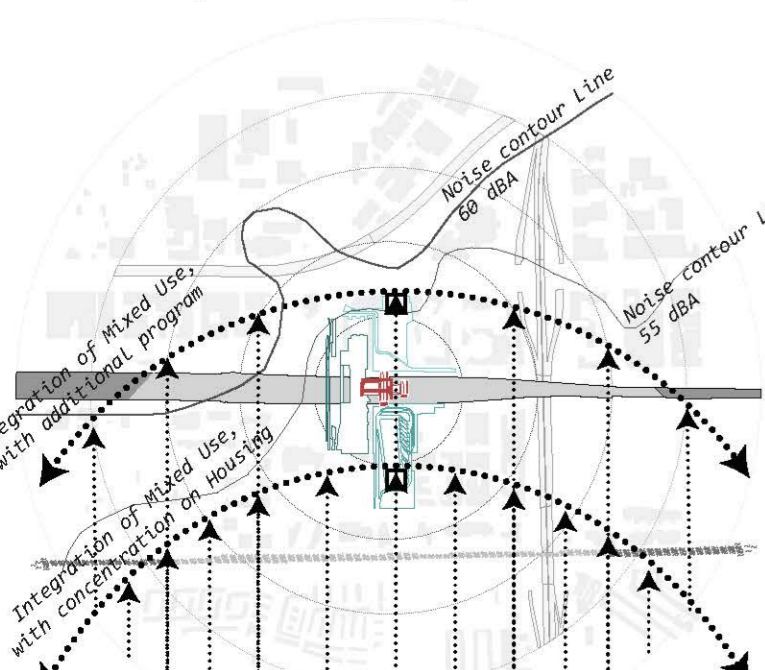
Sloterdijk Centrum in the near future



1:5000

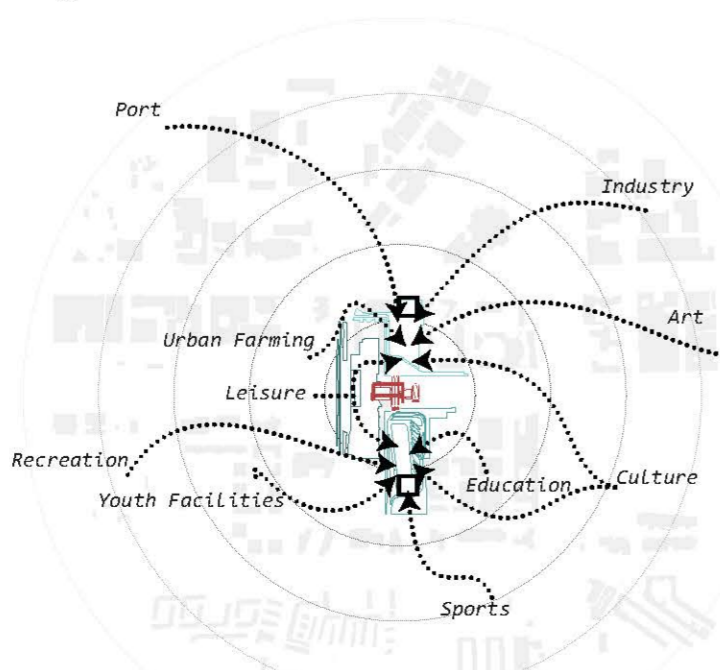
Strategic Maps - Intentions

Borders - Integration with the city



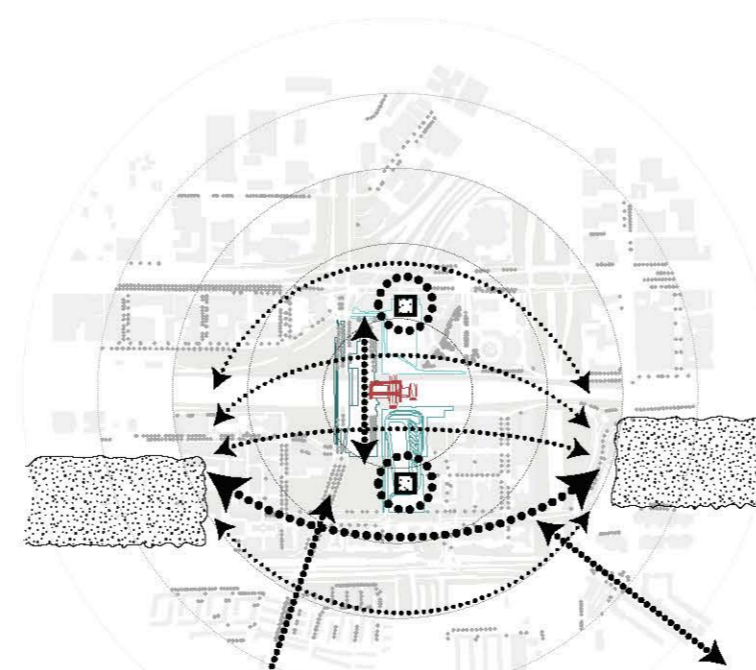
It is proposed a new system of integration with the city from the south. That happens by adding mixed residential program until the area of the station.

Programmatic Clusters



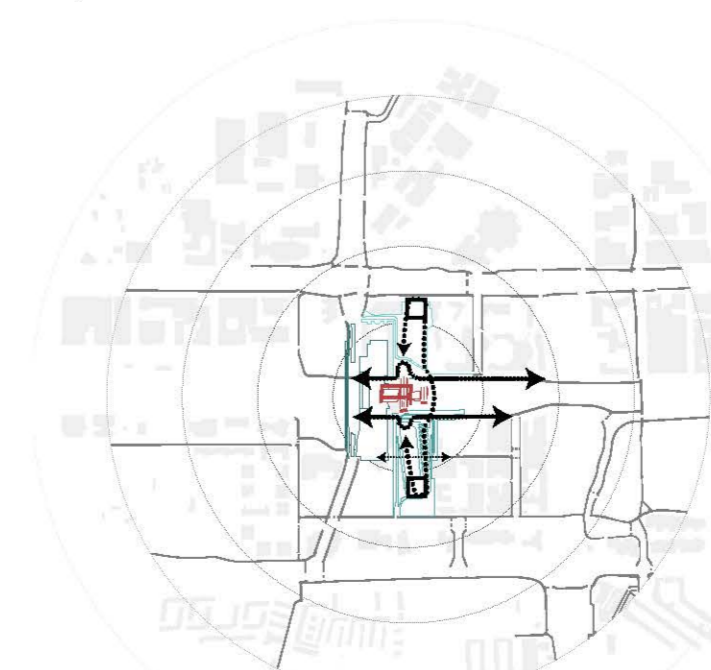
Because of the diversity of the area, it is adapted a strategy, mirroring and knitting of public programs that exist already in the great area of Amsterdam West.

Green



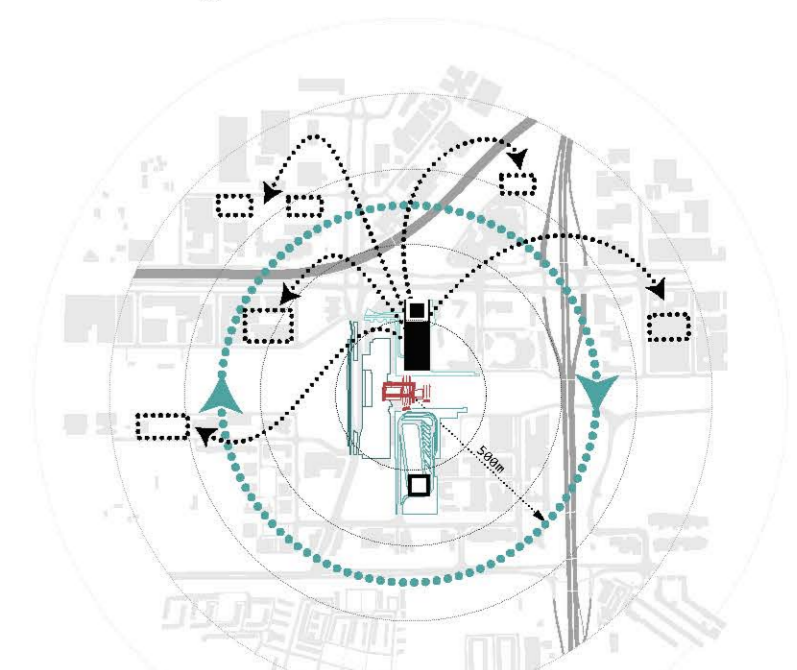
The two big green areas (Westerpark and Spieringhoren) are connected through green corridors that accompany the new program but also there are new green connections with the urban areas.

Bicycles & Pedestrians



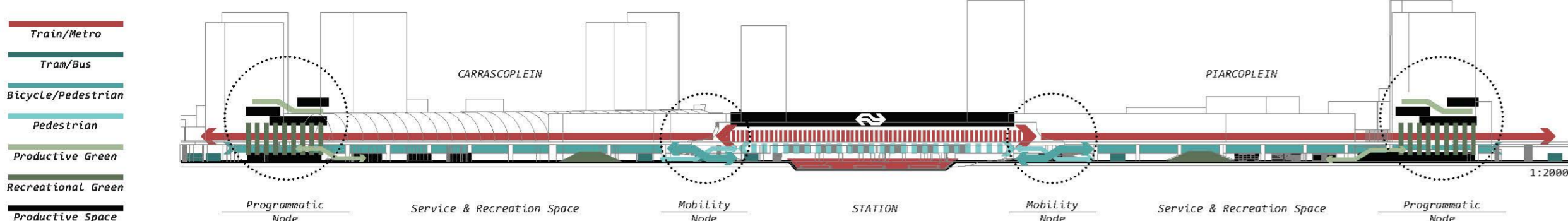
New developments bring more need for transaction and green infrastructure. Therefore it is proposed a more integrated system for bicycles and pedestrians.

Cars & Parking



It is proposed the ParkRide at Piarcopein, to be removed in different locations around the station in a walking distance. Consequently, it is proposed a more sustainable development around the station and also encouraged new car sharing systems and of course public transport.

Leaving space - Creating Space



The Urban Spine

Focusing on the train station, as the primal factor of generator of the urban environments in the area, we develop a strategy, between the three existing urban squares. The pedestrian and cycling flows and functions meet the transition that happens firstly through the station and secondly through elements of space that need to be adjusted and integrated in the urban fabric. The new developments in the area and the goals for the future (Havenstad 2040), impose a new character that creates a fertile ground for urbanity and form the base for a long-term vitality.

The strategic long-term design, regarding the new connections, negotiate between those elements that can be fixed and integrated (pedestrian bridge) and those elements that need to stay flexible to anticipate future changes (new station, new tracks etc.)

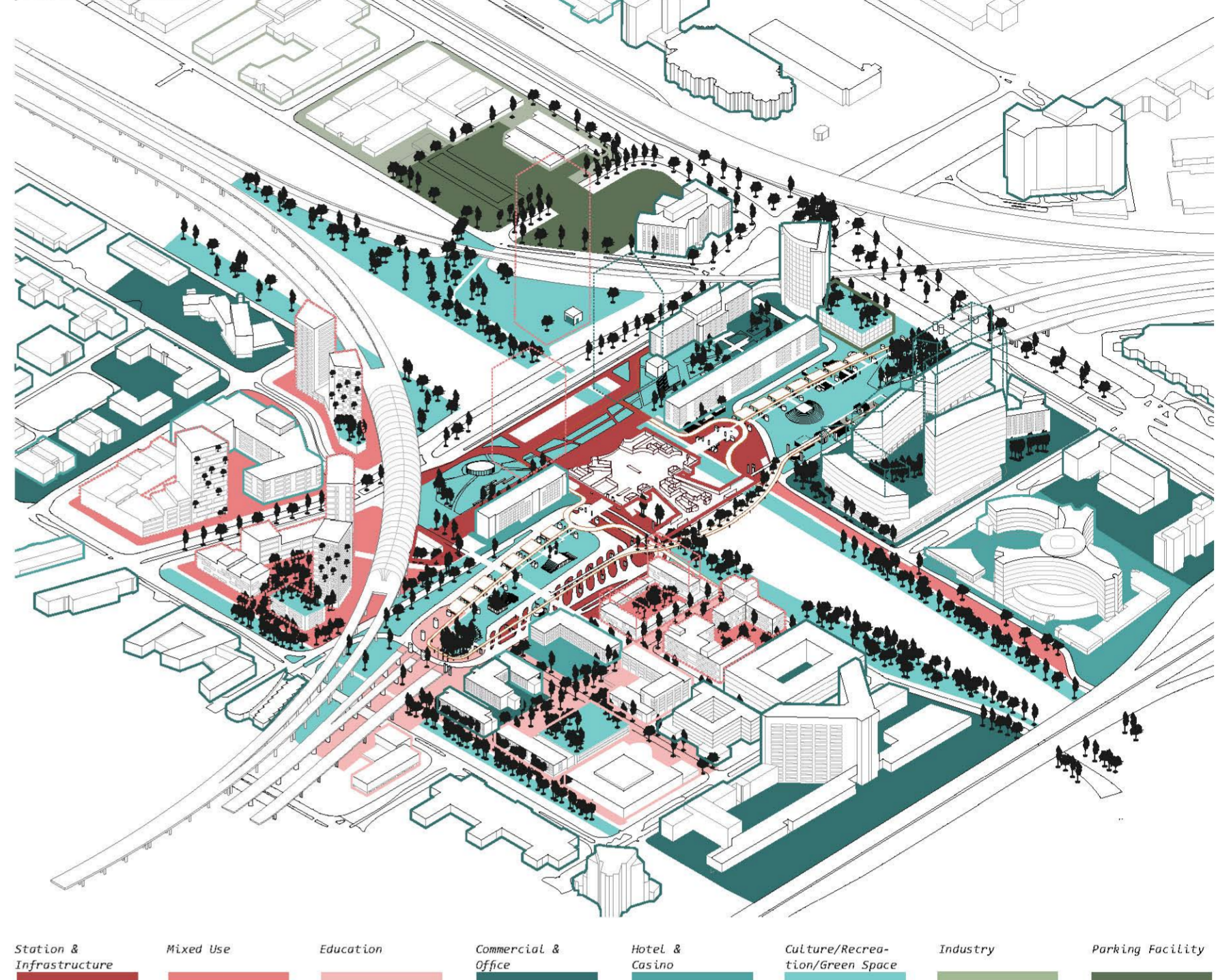
Therefore, it is created a new identity that is based on the continuation of the surroundings and street networks but also reinforced by new programmatic clusters, green and new visual connections. The changing programmatic needs (new housing, casino, Media College etc.) and socio-economic circumstances are also taken into consideration so to create a flexible public space. There are not unattractive entrances in the area or undefined urban edges difficult to inhabit. By "leaving space & creating space" we avoid fragmentation and turn the adjacent program towards the new "urban spine".

The new breathing space, starts to connect fragmented and forgotten areas around the station and make them vibrant and liveable once again. The result is a strong urban identity that allows a sustainable urban lifestyle.

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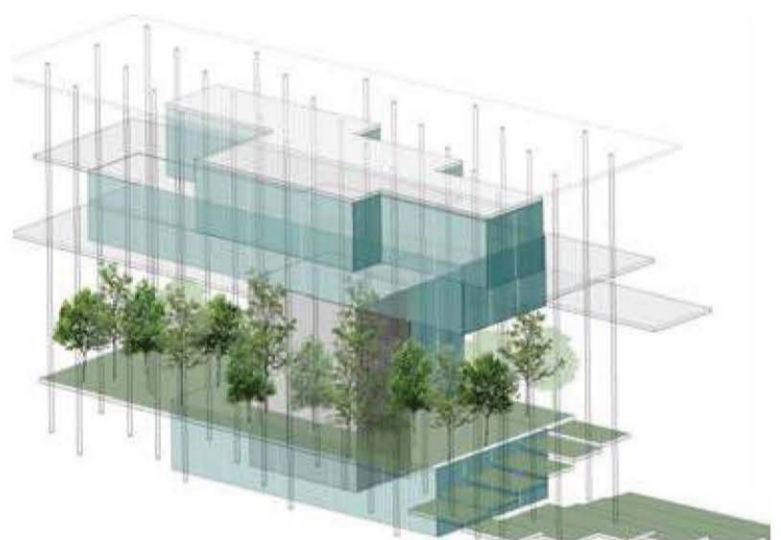
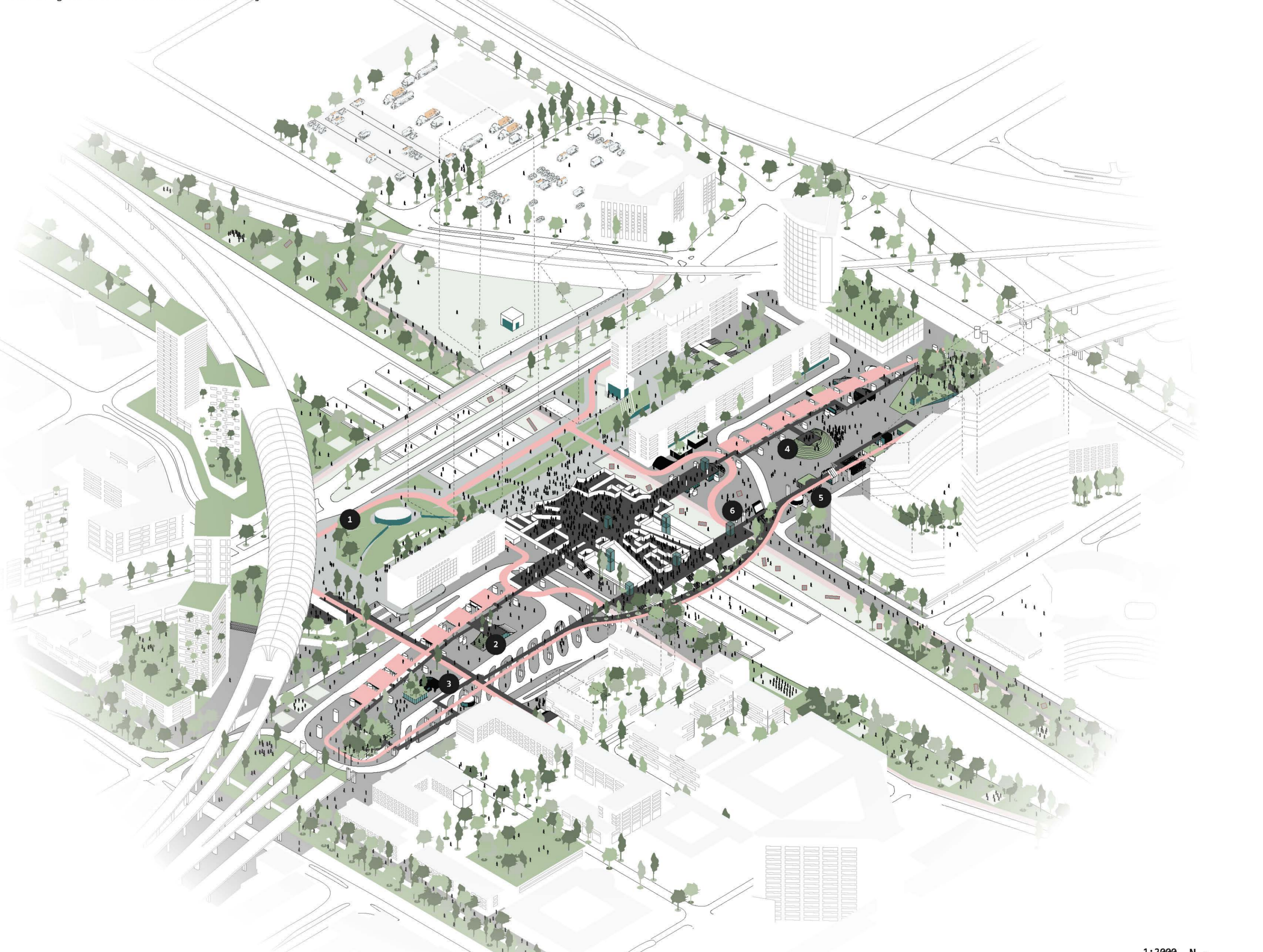
Programmatic Clusters



The two "stairways"

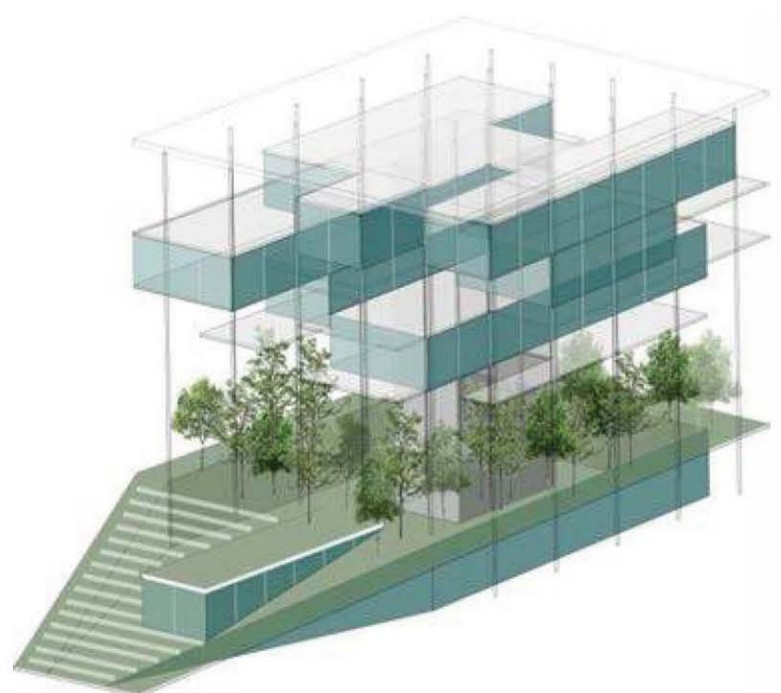
Following the example of Orlyplein with Bret and Hermes acting as satellites for urban activity, surrounding the main public space, the new urban solutions for the area suggests two metaphorical but also literal "stairways" around Piarcopelein and Carrascopelein. Based on the urban strategies, they become part of the proposed connections and they create a flexible qualitative space between the station and the edges of each square. Combined with program, the two modular and temporary structures, suggest a way of redefining the urban space and they act as catalysts for inclusiveness in knowledge, food and sociality. The temporality of the structures is "framed" vertically by a massive green volume, the urban forest.

Re-knitting the network around Amsterdam Sloterdijk



Cultural Stairway

At the edge of Carrascopelein, the volume of knowledge and education stands dominant between the rails, offering new and experimental ways of learning. With a modular and temporary character, it facilitates autonomy, interaction and knowledge exploration. The cultural stairway is a microcosmos of the digital and creative city.

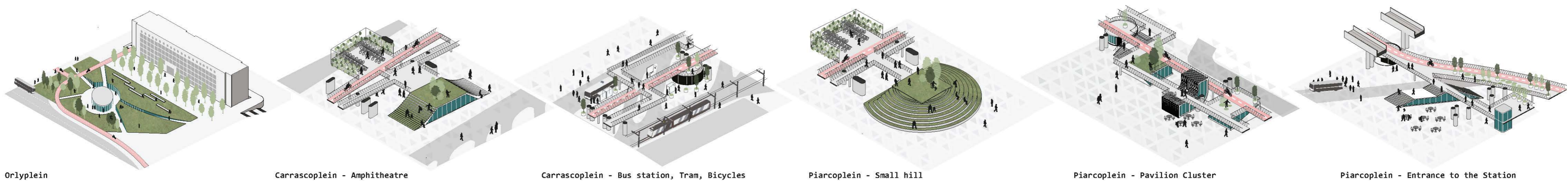


Green Stairway

Piarcopelein has the particularity of being in the end of the area, known as Sloterdijk Centrum. Therefore, a green stairway is proposed which intends to generate activity and act as an incubator not only for Sloterdijk, but the whole Amsterdam West. Providing spaces for vertical urban farming, it is linked with production and sustainability. The structure which can get support by local people, the municipality and the plant production industry, exposes modern food technology and agriculture to the consumer and aims to familiarize the public with its function. The green stairway is a microcosmos of the green and healthy city.

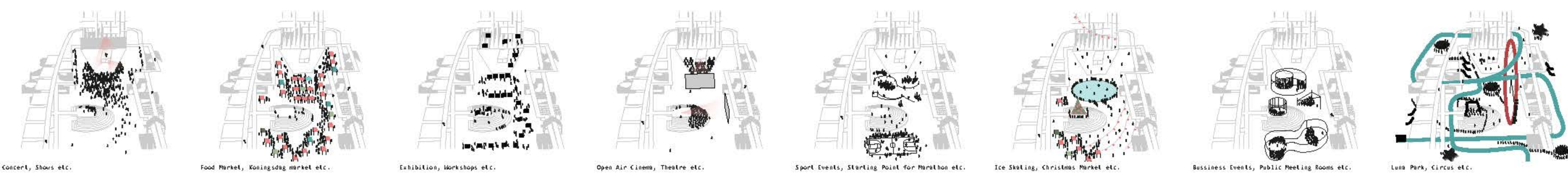
Public Space

- Orlyplein is an exceptional example that requires no extensive interventions. The new plan adds extra spaces that reinforce the character of the area and the two satellites (Bret & Hermes) but also the train station. Following its successful story, the proposed program focuses on leisure. The grassy and flowerish character is maintained but the patches are smoothly elevated in order to add spaces underneath and create diverse recreational sitting areas.
- The amphitheatre acts as a node along the elevated path that is created for bicycles and pedestrians. It is a new sustainable multifunctional and open space, combined with additional program. It acts as vertical connection in the human scale, contradicting the high rises and the big developments in the area. Here again, green is not an isolated element of the landscape, but is part of the functionality and the vitality that Carrascopelein requires.
- Car-rascopelein is concentrated on mobility and this should not change. The urban connections and the new pedestrian and bicycle routes, enrich this character by avoiding spatial fragmentation. The existing urban fabric and urban void are mixed with the new, whenever possible, while new program which is related with the character of the square (administration office for the bus company, tourist information, office spaces or food-to-go pavilions) is added.
- The small hill in Piarcopelein acts as a podium that re-organizes the space and activates the surroundings. The green volume is intentionally positioned in the middle of the square, posing a strong image of inclusiveness. The object allows people to see and be seen, to speak and to listen, to show and to hide. Therefore temporary program such as open-air cinema, theatre, urban games etc. are easily organized around it. The space underneath includes an artistic function.
- Piarcopelein facilitates clusters of pavilions that develop in an organic arrangement similar to a village's spatial organization. These small productive spaces with specific dimensions, bring back the small scale through their design, while they act as vertical connectors between the ground level and the new elevated path. Even though they are working and productive spaces, they have an exhibitional character and revitalize the dead space below the viaducts.
- Collour, leisure and adaptability characterize the new entrances to the train station. The upcoming Local developments reflect on the proposed open public space. A green route with new functions substitutes the mono-function of the current pedestrian bridge, cultural and artistic activities along the rails are suggested and space is created for accommodating car-sharing tendencies and the touristic buses. Before taking the urban stairs, one can grab a quick coffee underneath of them.



1:2000

"Architecturing" Events



Pavilion Guidelines

Seventeen pavilions articulate along the elevated urban route and compose the Tech Village in Piarcoplein. With various geometries, functions and a random arrangement, the pavilions re-introduce the spatial qualities of an organic city, such as Amsterdam's city center or Old Sloterdijk. Organized either in clusters or in linear sequences, part of them serves for the vertical circulation by reaching or extending above the level of the elevated route. In spite of the big diversity in shapes and programs, it is highly important that these structures are using the same design language. In other words, a unified design approach, consistent in several aspects is considered essential. Therefore a series of design guidelines is being set to secure a homogenous and recognizable urban image.

In the present plan for Piarcoplein, each pavilion has been addressed and designed individually by the authors. Our aim though is to use these structures as an open invitation for young architects and architecture students to unfold their imagination and "design for Sloterdijk". Being introduced as an architectural competition, this initiative will trigger the development of Piarcoplein by drawing the attention of young design practices, artists, entrepreneurs, and innovative academic institutions. In such case, the presence of design guidelines for the pavilions is even more crucial as a limitless design freedom will result in a scatter outcome. The participants can only enter the design context by accepting their compliance to the following guidelines.

is mainly concentrated in the ground level.

2. **Materials**  
Aiming for circularity and low environmental impact for elements with a primarily temporary character, the pavilions must have a visible timber load-bearing structure while timber is suggested to be exposed also at the facades as structural or non-structural element (cladding). To ensure light quality and ventilation of the interiors, a minimum surface of 50% of the entire wall surface must be transparent glass. Finally, in order to improve the environmental profile of the structures and add green in Piarcoplein, green roofs are highly recommended. However, these should be added only if they are accessible to people either from the ground level or from the elevated route. Thus, the quality of the pedestrian route is enriched with green areas in which urban furniture is added.

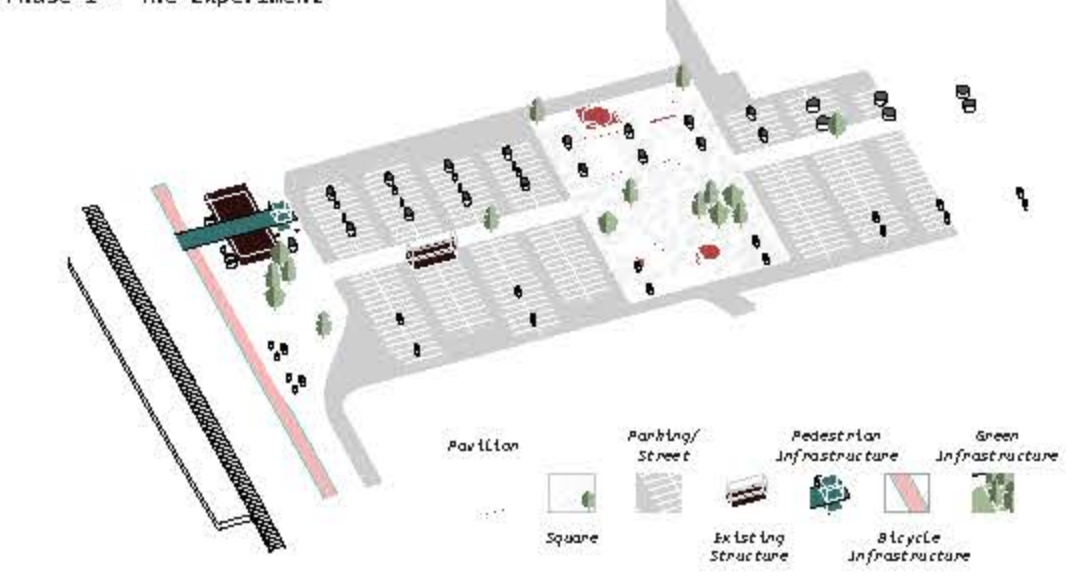
3. **Program**  
"The best way to create value in the 21st century is to connect creativity and technology", according to Apple's former CEO, Steve Jobs. The added program relates to a range of functions that target in productivity from technological and creative groups. Flexible office spaces are offered to start-ups and studios to creative artists (photography studio), fabrication laboratories (fab labs) are provided with small-scale workshops to apply digital manufacturing techniques (3D printing, robotic milling, CNC cutting, etc) while industrial design manufacturers (bicycles, furniture, etc) with small scale equipment fit in the character of the Tech Village as well. Exhibition spaces are added in order to expose the work of the village to the public and bring the various groups together.

Regardless of the development of new pavilion designs by the participants, the pavilion as a design subject is attractive to the authors as well. Therefore, in the case of a full realization, a minimum number of three pavilions should be implemented in accordance to the author's drawings.

1. **Dimensions**  
The structures must occupy area of not larger than 100sqm that fit in a rectangular space of 10x10m. The locations of which are given. Extension in a second level is allowable without exceeding the maximum allowable area. An ideal proportion in that case is 70% of the total area in the ground floor and 30% as an additional level. In this way new program

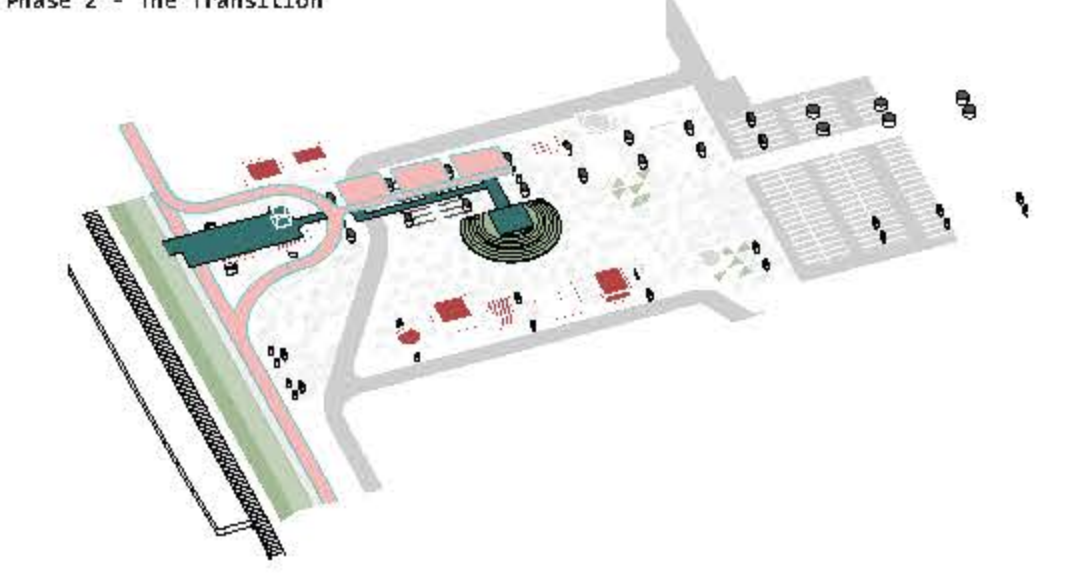
Phasing

Phase 1 - The Experiment



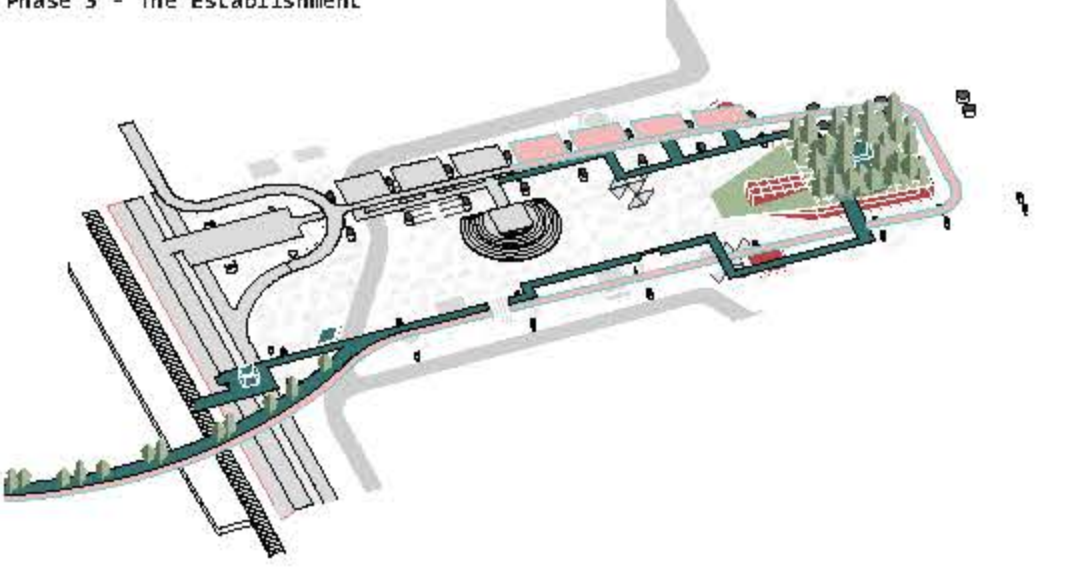
The overall development plan and transformation of Piarcoplein is smoothly implemented into four phases. The experiment begins with the removal of 30% of the current parking places at the middle of the square which opens space for urban activity. Meanwhile, the first six pavilions that are implemented on that area give birth of the new situation. Installation of trees completes the image of this first set. Gradually car users realize that the P&R in Sloterdijk is changing.

Phase 2 - The Transition



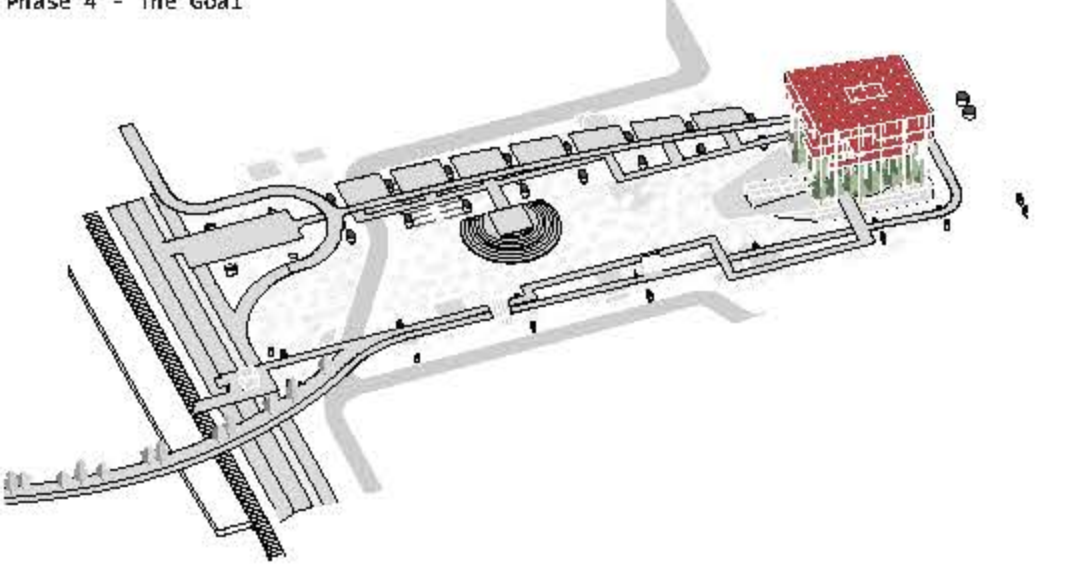
Given the success of the first approach, another 40% of the total parking lots is removed, connecting spatially the first experimental space with the station. Ten more pavilions are installed, adding program to the area. Apart from the light temporary structures, the infrastructural plan is introduced as well. The elevated urban route is partly constructed, connecting directly Piarcoplein with Orlyplein, while the bicycle parking opens with half of the total parking spaces of the final plan. Green is increased in the square with the implementation of the green hill which is an important element of the future life of Piarcoplein.

Phase 3 - The Establishment



During realization of the third phase the remaining car parking places are removed, giving their place to a vital ingredient, the urban forest. Standing on top of large space dedicated to food production, the forest consists of numerous trees that absorb noise and air pollution while creating a green wall between the square and the adjacent highway. In parallel, the elevated path is completed together with its vertical circulation, offering a new connection of Piarcoplein with the two neighbouring squares. The last three pavilions are installed, bringing the square closer to its final shape.

Phase 4 - The Goal



Having already a big number of productive spaces created during the previous stages, Piarcoplein receives its ultimate image with the addition of the Green stairway. This modular tower consists of prefabricated mobile spaces that are assembled on location on top of the urban forest.

The Instruments

- 1 Green Laboratory
- 2 Lecture Hall
- 3 Fab Lab
- 4 Parking for Disabled
- 5 Drop-off point
- 6 Cafe
- 7 Storage and Circulation
- 8 Bike Lab
- 9 Sculpture Passage
- 10 Green Bridge
- 11 New Vertical Circulations
- 12 Active Hotel Edges
- 13 Bicycle Parking
- 14 Green Accessible Roof
- 15 Going-through Roof
- 16 Slanted Sitting Roof
- 17 Urban Forest
- 18 Circulation Core
- 19 Vertical Urban Farming
- 20 Solar Panel Roof

