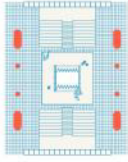


The theatre

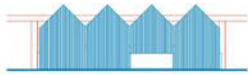
A place for showcasing, gathering, community and events. Where the next business deal is closed and the city hangs out in the evening.



25 m

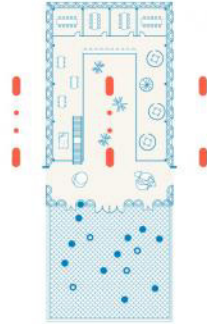
The warehouse

A grandiose space capable of accommodating any kind of production experiment. With total flexibility and shared facilities it instigates innovation and creation.



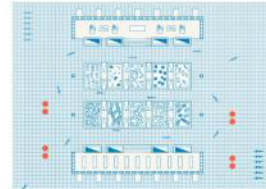
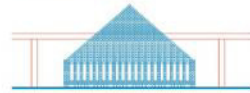
The gallery

Meetings, working space, tourist information, art and culture come together in front of the central square, giving out onto the kiss & ride.



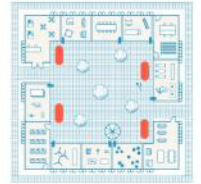
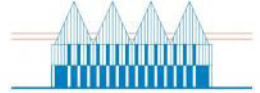
The resource centre

Piarco Village's supermarket with a supply chain originating in the harbour and the city. Here, things are taken apart and redistributed to give them a new life.



The lab

A more intimate setting of production spaces around a shared courtyard, ready for the next high tech solution to be developed.



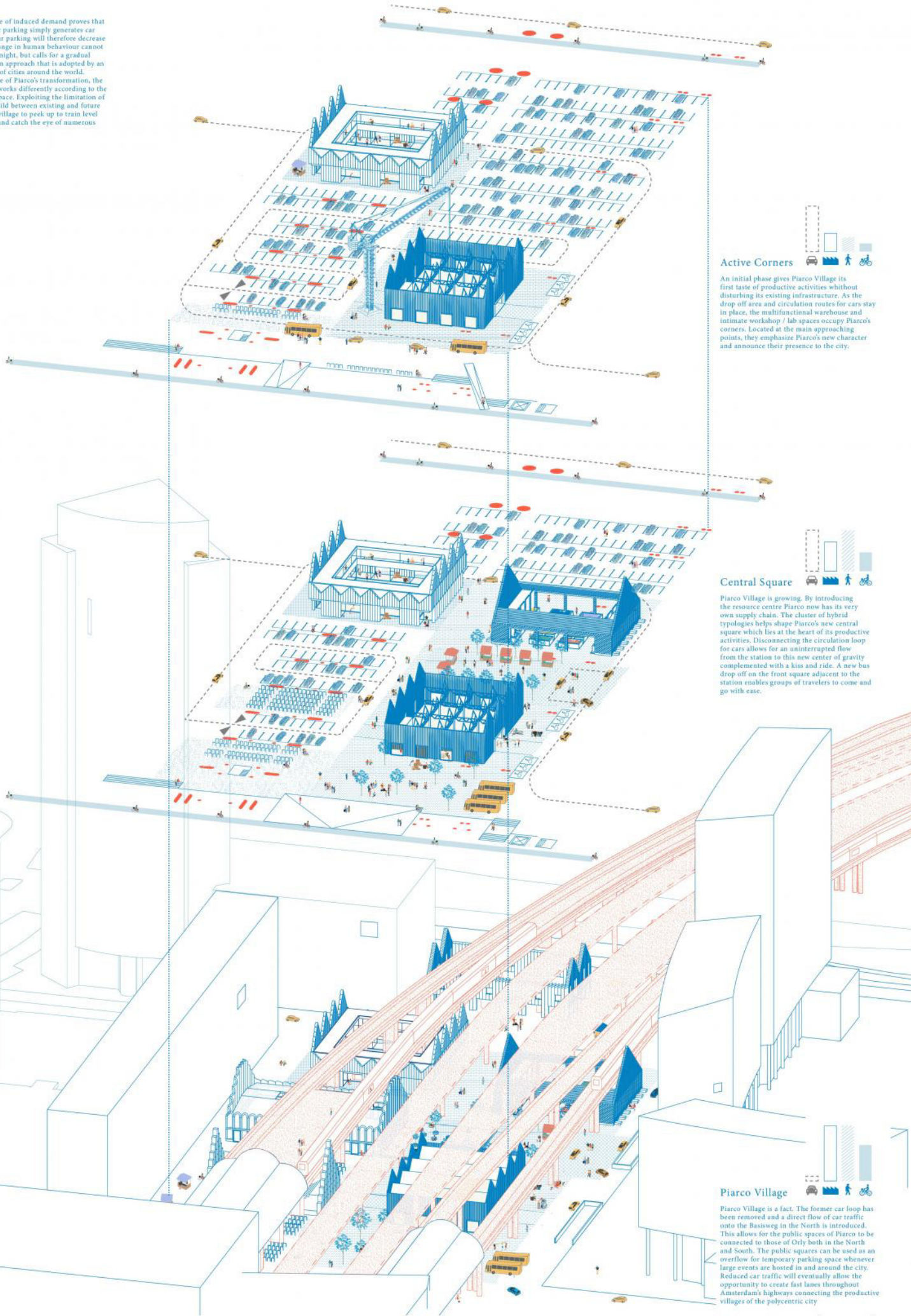
Hybrid typologies - a symbiosis of the civic world and the productive world as we know it - will populate Piarco Village. These typologies provide spaces in a variety of shapes and scales, a permanent base capable of adapting to an extremely flexible productive programme.

Word will spread soon, establishing a platform for Piarco's users to showcase their productive experiments. Existing initiatives are empowered while new ones are initiated - Fairphone, Scrap Connection and Vandebrom were only the beginning. A new strong visual identity and a highly productive programme counterweight Piarco's surrounding blandness and anonymity with a joy for life that was previously unimaginable.



The principle of induced demand proves that the provision of car parking simply generates car traffic. Reducing car parking will therefore decrease car traffic. This change in human behaviour cannot be established overnight, but calls for a gradual implementation - an approach that is adopted by an increasing amount of cities around the world.

In each phase of Piarco's transformation, the productive village works differently according to the extent of parking space. Exploiting the limitation of opportunities to build between existing and future bridges allows the village to peek up to train level to announce itself and catch the eye of numerous bypassers.



**Active Corners**

An initial phase gives Piarco Village its first taste of productive activities without disturbing its existing infrastructure. As the drop off area and circulation routes for cars stay in place, the multifunctional warehouse and intimate workshop / lab spaces occupy Piarco's corners. Located at the main approaching points, they emphasize Piarco's new character and announce their presence to the city.

**Central Square**

Piarco Village is growing. By introducing the resource centre Piarco now has its very own supply chain. The cluster of hybrid typologies helps shape Piarco's new central square which lies at the heart of its productive activities. Disconnecting the circulation loop for cars allows for an uninterrupted flow from the station to this new center of gravity complemented with a kiss and ride. A new bus drop off on the front square adjacent to the station enables groups of travelers to come and go with ease.

**Piarco Village**

Piarco Village is a fact. The former car loop has been removed and a direct flow of car traffic onto the Basisweg in the North is introduced. This allows for the public spaces of Piarco to be connected to those of Orly both in the North and South. The public squares can be used as an overflow for temporary parking space whenever large events are hosted in and around the city. Reduced car traffic will eventually allow the opportunity to create fast lanes throughout Amsterdam's highways connecting the productive villages of the polycentric city.

# Towards a poly-centric productive city

Like many European cities, Amsterdam started out as a village in support of a local economy. Over time, the village and harbour developed together, overlapping and intertwining: the canals served the ships while providing a mode of transportation; the square in front of the town hall hosted the daily market; the blacksmith's workshop sat alongside the local theatre. The city was a productive world capable of sustaining itself.

This marriage of city and production came to an end. The construction of the Noordzee Kanaal in 1876 saw the harbour moving westwards - out of the city - gradually decreasing its level of production and becoming more just a vessel for goods. Globalisation - encouraged by the introduction of Schiphol Airport after WW1 - encouraged the city of Amsterdam to reinvent itself as a creative knowledge economy. Today, city, harbour and airport make up Amsterdam's economic landscape, but their geographical divorce prevents interaction, and this comes at a price. For example, energy supplies for electricity and heat are import and fossil fuel dependent, recycled waste is down-cycled, and an average dinner has travelled 30,000 kilometres. Amsterdam now functions as a three-poled non-productive city.

So how do we turn the tide? By exploiting the incredible potential of Amsterdam's rail and road infrastructure. This existing network - with its nodes and left over spaces - presents itself as the ideal carrier for re-connecting city, harbour and airport. A network of productive villages, each with its own character, resulting in a polycentric productive city capable of re-introducing Amsterdam's circular economy.

With a daily passenger rate of 50,000 people, Piarcoplein forms the ideal test ground for the first productive village. Today, the square consists of a landscape of infrastructures, a series of beautiful columns and bridges. Celebrating these two key elements as catalysts for spatial layout, Piarco's productive village weaves around and between columns and bridges, breaking up the site's vastness and introducing a sequence of human scale landscapes.



100 m

